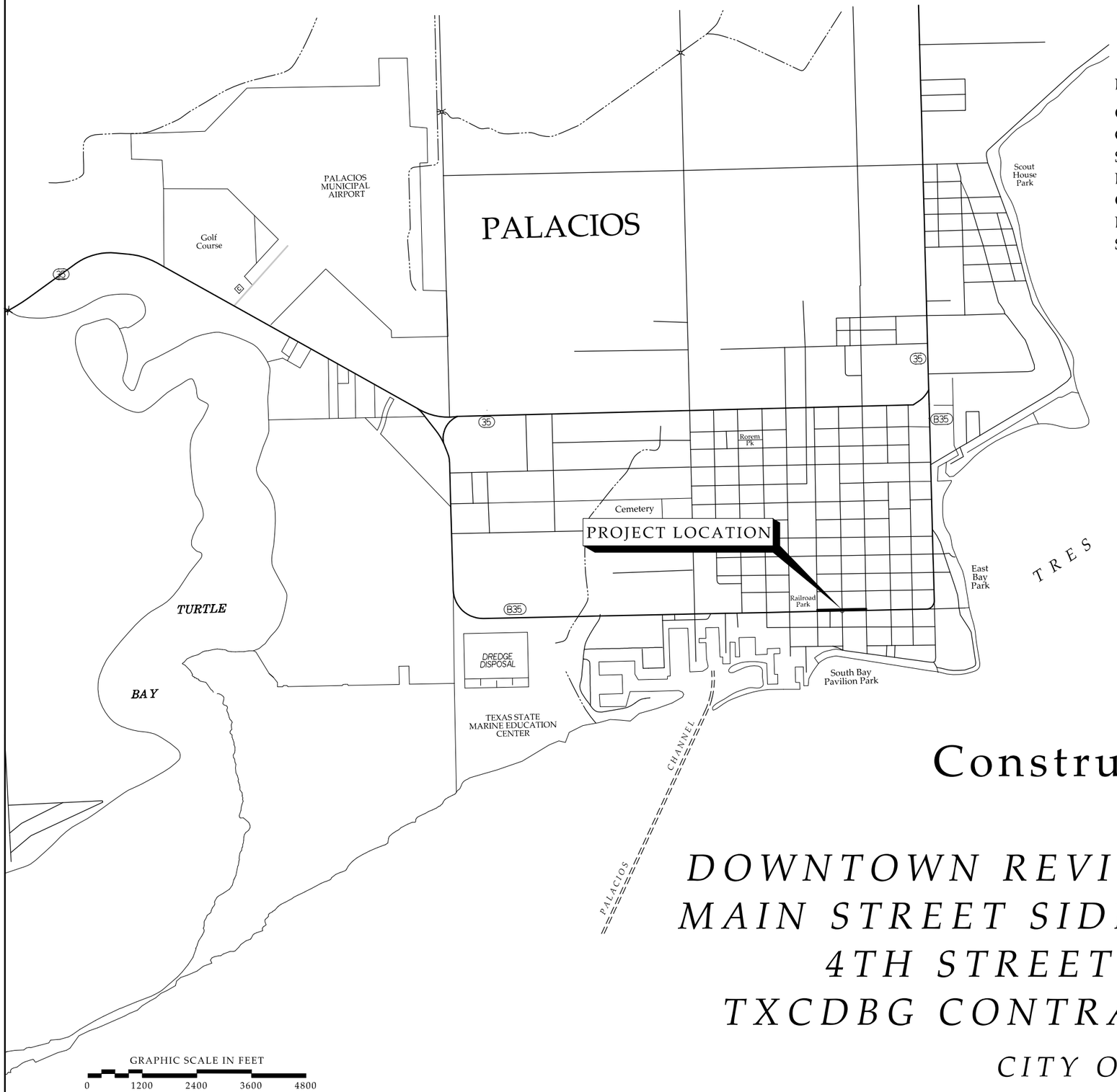

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Construction Plans For

DOWNTOWN REVITALIZATION PROJECT
MAIN STREET SIDEWALK REPLACEMENT
4TH STREET TO 6TH STREET
TXCDBG CONTRACT NO. CDM21-0045

CITY OF PALACIOS

STATE OF TEXAS
MATT A. GLAZE
99253
LICENSED
PROFESSIONAL ENGINEER
2/14/23

ISSUE
- [ ] PRELIMINARY
- [ ] REVIEW
- [ ] PERMITTING
- [x] BIDDING
- [ ] CONSTRUCTION
- [ ] RECORD DRAWING

REVISIONS

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PROJECT
CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE
COVER

DATE 02/14/23

JOB NUMBER E23940.00

SHEET
C1

PAGE 1 *of* 14

GENERAL NOTES

PRELIMINARY MATTERS

1. THE INSTRUCTIONS GIVEN BY THE NOTES ON THIS SHEET DO NOT CONSTITUTE SEPARATE PAY ITEMS UNLESS SPECIFICALLY INCLUDED IN THE PROPOSAL FORM.

2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS LISTED IN THE CONTRACT DOCUMENTS & THE STANDARD DETAILS INCLUDED OR REFERENCED IN THE PLANS.

3. ANY CHANGES OR REVISIONS TO THESE PLANS MUST BE SUBMITTED TO THE ENGINEER FOR REVIEW & APPROVAL PRIOR TO IMPLEMENTATION.

4. THE LOCATIONS OF EXISTING UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE & WERE OBTAINED FROM EXISTING RECORDS & VISIBLE EVIDENCE ON THE GROUND. IT IS EXPECTED THAT THERE MAY BE SOME DISCREPANCIES & OMISSIONS IN THE LOCATIONS & QUANTITIES OF EXISTING UTILITIES & STRUCTURES SHOWN. THE CONTRACTOR SHALL VERIFY THE LOCATION & DEPTH OF ALL KNOWN EXISTING UTILITIES SUFFICIENTLY IN ADVANCE OF CONSTRUCTION SO THAT CONFLICTS CAN BE AVOIDED. WHEN AN EXISTING UTILITY OR UNDERGROUND PIPELINE IS ENCOUNTERED, THAT WAS PREVIOUSLY NOT LOCATED OR INCORRECTLY LOCATED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER & THE APPROPRIATE UTILITY COMPANY TO OBTAIN PROCEDURAL INSTRUCTIONS. THE CONTRACTOR SHALL COOPERATE WITH THE APPROPRIATE UTILITY COMPANY IN MAINTAINING ACTIVE SERVICES IN OPERATION.

5. EXISTING PAVING, BUILDINGS & OTHER ITEMS SHOWN ON PLANS, BUT NOT SPECIFICALLY RELATED TO THE WORK OF THE CONTRACTOR, ARE FOR INFORMATIONAL PURPOSES ONLY & MAY BE SHOWN TO A LESSER ACCURACY OR TO A LESSER DEGREE OF DETAIL THAN THE REMAINDER OF THE PLANS.

6. ELEVATIONS SHOWN ON THE PLAN & FOLLOWED BY A "±" SYMBOL, INDICATE THAT THE ENGINEER'S INTENTION IS TO MATCH THE EXISTING GRADE OF THE TIE-IN PAVEMENT OR STRUCTURE. THE CONTRACTOR SHALL VERIFY THE ELEVATION AT THESE LOCATIONS & NOTIFY THE ENGINEER IMMEDIATELY, IF THE PLAN ELEVATION VARIES SIGNIFICANTLY.

7. WHERE ELEVATIONS ARE SHOWN ON THE PLAN AS "TBD", IT INDICATES THAT THE ELEVATIONS ARE **TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR**. THIS NOTATION IS TYPICALLY USED FOR BURIED UTILITIES WHO'S ELEVATION COULD NOT BE DETERMINED BY AS-BUILT PLANS, OR PROBING DURING THE DESIGN PHASE OF THE PROJECT. THE CONTRACTOR SHALL EXCAVATE THE UTILITY, DETERMINE THE ELEVATION, AND NOTIFY THE ENGINEER IMMEDIATELY, SO THAT ADJUSTMENTS MAY BE MADE TO THE DESIGN PRIOR TO ORDERING MATERIALS OR SCHEDULING THE WORK.

8. THE OWNER/ENGINEER RESERVE THE RIGHT TO MAKE REASONABLE ADJUSTMENTS IN LINE AND/OR GRADE IN ORDER TO AVOID CONFLICTS WITH NON-RELOCATABLE STRUCTURES OR OTHER UTILITIES. THE CONTRACTOR AGREES TO MAKE SUCH REASONABLE ADJUSTMENTS AT NO COST TO OWNER OR ENGINEER.

9. EXISTING ELECTRICAL LINES ARE LOCATED CLOSE TO THE PROJECT. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO THE STATE LAW (VERNON'S ANNOTATED TEXAS STATUTES, ARTICLE 1436(C)) CONCERNING OPERATIONS IN THE VICINITY OF ELECTRICAL LINES & THE NEED FOR EFFECTIVE PRECAUTIONARY MEASURES.

10. THE MUNICIPALITY SHALL PERFORM ALL OPERATION INVOLVING OPENING & CLOSING OF VALVES ON EXISTING PUBLIC WATER MAINS. THE CONTRACTOR SHALL VERIFY MAINS ARE DEAD BEFORE PERFORMING WORK ON EXISTING MAINS.

NOTIFICATION REQUIREMENTS

1. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO THE OWNER, ENGINEER & PERSONS IN CHARGE OF PRIVATE & PUBLIC UTILITIES AFFECTED BY HIS OPERATIONS PRIOR TO COMMENCEMENT OF WORK.

2. AT LEAST 48 HOURS PRIOR TO COMMENCING ANY ACTIVITY FOR A TCEQ REGULATED SANITARY SEWER AND/OR WATER COLLECTION SYSTEM(S), THE CONTRACTOR SHALL NOTIFY THE LOCAL TCEQ's REGIONAL OFFICE, IN WRITING, OF THE DATE ON WHICH CONSTRUCTION WILL BEGIN.

3. AT LEAST 48 HOURS PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR IS REQUIRED TO NOTIFY TEXAS ONE CALL AT 1-800-245-4545.

4. THE CONTRACTOR SHALL NOTIFY LOCAL EMERGENCY SERVICES (I.E. FIRE, E.M.S. & POLICE) OF ANY CONSTRUCTION ACTIVITIES THAT WOULD AFFECT THE NORMAL FLOW OF TRAFFIC.

5. THE CONTRACTOR SHALL GIVE A MINIMUM OF 48 HOURS NOTICE TO THE ENGINEER & AUTHORIZED TESTING LABORATORY PRIOR TO REQUIRED TESTS.

6. THE CONTRACTOR SHALL GIVE A MINIMUM OF 48 HOURS NOTICE TO THE ENGINEER & THE OWNER PRIOR TO TESTING OF SANITARY SEWER & WATER LINES. INSPECTION BY THE MUNICIPALITY IS REQUIRED FOR ALL TESTING OF SANITARY SEWER & WATER LINES.

CONTRACTOR'S RESPONSIBILITIES

1. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER & THE ENGINEER OF ANY DISCREPANCIES, ERRORS, OR OMISSIONS, DISCOVERED IN THE FIELD OR ON THE PLANS.

2. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER & THE ENGINEER, VERBALLY & IN WRITING, OF ANY FUEL OR TOXIC MATERIAL SPILLS ONTO THE PROJECT/CONSTRUCTION AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DISPOSING OF FUELS, WASTE MATERIALS & CONTAMINATED EXCAVATIONS IN A LEGALLY APPROVED MANNER.

3. THE CONTRACTOR SHALL COORDINATE INTERRUPTIONS OF ALL UTILITIES & SERVICES WITH APPLICABLE UTILITY COMPANY, OWNER & TENANT. ALL WORK SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE APPLICABLE UTILITY COMPANY OR AGENCY INVOLVED.

4. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING INGRESS & EGRESS FOR ALL PUBLIC & PRIVATE FACILITIES AT ALL TIMES & FOR ALL WEATHER CONDITIONS, UNLESS OTHERWISE INDICATED ON THE PLANS OR APPROVED BY THE ENGINEER.

5. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE & MAINTAIN ALL NECESSARY WARNING & SAFETY DEVICES (FLASHING LIGHTS, FLAG MEN, BARRICADES, SIGNS, ETC.) TO PROTECT THE PUBLIC SAFETY & HEALTH UNTIL THE WORK HAS BEEN COMPLETED & ACCEPTED BY THE ENGINEER & OWNER. ALL BARRICADING SHALL BE DONE IN COMPLIANCE WITH THE TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

6. THE CONTRACTOR SHALL MAINTAIN ALL REGULATORY SIGNS DURING THE CONSTRUCTION PERIOD.

7. THE CONTRACTOR SHALL ASSURE HIMSELF THAT ALL CONSTRUCTION PERMITS HAVE BEEN OBTAINED PRIOR TO COMMENCEMENT OF WORK. REQUIRED PERMITS THAT CAN ONLY BE ISSUED TO CONTRACTOR ARE TO BE OBTAINED AT THE CONTRACTOR'S EXPENSE.

8. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING & MAINTAINING SANITARY FACILITIES ON THIS PROJECT FOR EMPLOYEES.

9. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE FLOW IN DITCHES & STORM SEWERS AT ALL TIMES.

GENERAL NOTES

CONTRACTOR'S RESPONSIBILITIES (CONT.)

10. THE CONTRACTOR IS RESPONSIBLE FOR DISPOSING OF ALL EXCESS CONSTRUCTION & WASTE MATERIALS. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE LOCAL, STATE, & FEDERAL REQUIREMENTS REGARDING HANDLING & DISPOSAL OF EXCESS & WASTE MATERIAL. THE CONTRACTOR SHALL PAY PARTICULAR ATTENTION TO AREAS AROUND CONCRETE PAVEMENT & STRUCTURES TO ENSURE THAT CONSTRUCTION DEBRIS IS REMOVED & PROPERLY DISPOSED OF PRIOR TO BACKFILLING & THE APPLICATION OF TOPSOIL. EXCESS SOIL, ROCK OR SPOIL MATERIAL SHALL BE REMOVED FROM THE PROJECT SITE & DISPOSED OF BY THE CONTRACTOR AT HIS EXPENSE.

11. ALL CONSTRUCTION OPERATIONS SHALL BE ACCOMPLISHED IN ACCORDANCE WITH APPLICABLE REGULATIONS OF THE U.S. OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION. COPIES OF OSHA STANDARDS MAY BE PURCHASED FROM THE U.S. GOVERNMENT PRINTING OFFICE. INFORMATION & RELATED REFERENCE MATERIALS MAY BE OBTAINED FROM OSHA, 903 SAN JACINTO, AUSTIN, TEXAS.

12. DESIGN INSTALLATION, MAINTENANCE, & INSPECTION OF TRENCH SAFETY SYSTEMS SHALL BE IN ACCORDANCE WITH THE PROVISIONS OF EXCAVATIONS, TRENCHING & SHORING, FEDERAL OCCUPATION SAFETY & HEALTH ADMINISTRATION (OSHA) STANDARDS, 29CFR, PART 1926, SUBPART P, AS AMENDED, INCLUDING FINAL RULE, PUBLISHED IN THE FEDERAL REGISTER VOL. 209 ON TUESDAY, OCTOBER 31, 1989. TRENCH SAFETY SYSTEMS SHALL ALSO BE IN ACCORDANCE WITH TEXAS HEALTH & SAFETY CODE ANN., 756.021 (VERNON 1991).

13. THE CONTRACTOR SHALL TAKE ALL DUE PRECAUTIONS TO PROTECT EXISTING FACILITIES (INCLUDING BUILDINGS, STRUCTURES, ROADWAYS, PARKING AREAS, DRIVEWAYS, UTILITIES, ETC.) FROM DAMAGE. ANY DAMAGE TO EXISTING FACILITIES INCURRED AS A RESULT OF THE CONSTRUCTION OPERATIONS ARE TO BE REPAIRED IMMEDIATELY BY THE CONTRACTOR TO A CONDITION SIMILAR OR EQUAL TO THAT EXISTING BEFORE THE DAMAGE WAS DONE. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE FACILITY OWNER & THE ENGINEER AT THE CONTRACTOR'S EXPENSE.

14. THE CONTRACTOR SHALL LOCATE, PROTECT & MAINTAIN BENCHMARKS, MONUMENTS & CONTROL POINTS. THE CONTRACTOR SHALL RE-ESTABLISH DISTURBED OR DESTROYED ITEMS AT HIS EXPENSE. THE RE-ESTABLISHMENT SHALL BE PERFORMED UNDER THE DIRECTION OF A TEXAS REGISTERED PROFESSIONAL LAND SURVEYOR.

15. WATER NECESSARY FOR CONSTRUCTION SHALL BE PROVIDED & PAID FOR BY THE CONTRACTOR. THE CONTRACTOR SHALL ARRANGE FOR A METERED CONNECTION(S) & SHALL PROVIDE THE PROPER EQUIPMENT TO PREVENT CROSS-CONNECTION.

16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY CHARGES ASSOCIATED WITH TEMPORARILY SECURING OR TEMPORARILY RELOCATING POWER POLES THAT INTERFERE WITH THE CONSTRUCTION OPERATIONS. THIS DOES NOT APPLY TO THE PERMANENT RELOCATION OF POWER POLES THAT ARE PHYSICALLY IN CONFLICT WITH THE PROPOSED IMPROVEMENTS.

17. THE CONTRACTOR SHALL CLEAR STREETS, SIDEWALKS, DRIVEWAYS, & PARKING LOTS OF ALL CONSTRUCTION MATERIALS, EQUIPMENT, TRAFFIC CONTROL DEVICES, DIRT, & DEBRIS CAUSED BY CONSTRUCTION AT THE END OF EACH CONSTRUCTION PERIOD. ALL OPEN EXCAVATIONS & PITS MUST BE BARRICADED, FENCED, OR PLATED OVER WHEN NOT IN USE.

18. GRAVITY MAINS SHALL BE INSTALLED IN THE UPSTREAM DIRECTION, BEGINNING AT THE LOWEST POINT IN THE SYSTEM. THE CONTRACTOR IS REQUIRED TO VERIFY THE LOCATION, ELEVATION & CONDITION OF THE ALL CONNECTION POINTS (I.E. UPSTREAM AND DOWNSTREAM) & INVESTIGATE ALL POTENTIAL CONFLICTS WITH EXISTING UNDERGROUND UTILITIES, PRIOR TO BEGINNING THE NEW UTILITY INSTALLATION.

19. UTILITY MAINS MUST BE INSTALLED WITH ADEQUATE COVER TO PREVENT FLOATATION & TO SUPPORT CONSTRUCTION LOADS. THE CONTRACTOR SHALL ENSURE THAT ADEQUATE COVER IS MAINTAINED OVER THE UTILITY DURING CONSTRUCTION. IF ADEQUATE COVER CANNOT BE MAINTAINED, THE CONTRACTOR SHALL UTILIZE CEMENT STABILIZED BACKFILL AND/OR ADDITIONAL TEMPORARY OVERBURDEN TO ACHIEVE THE SAME GOALS.

20. THE CONTRACTOR SHALL PLACE & COMPACT BACKFILL AS PROMPTLY AS PRACTICAL AFTER COMPLETION OF EACH STRUCTURE OR PORTION OF A STRUCTURE. DO NOT, HOWEVER, PLACE BACKFILL AGAINST NEWLY CONSTRUCTED CONCRETE WALLS OR SIMILAR STRUCTURES UNTIL CONCRETE HAS CURED AT LEAST 7-DAYS.

21. UNLESS OTHERWISE NOTED ON PLANS OR IN SPECIFICATIONS, THE CONTRACTOR SHALL PLACE & COMPACT BACKFILL AROUND UTILITY STRUCTURES IN ACCORDANCE WITH APPLICABLE TRENCH ZONE BACKFILL DETAIL FOR UTILITY LINE.

22. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION PRIOR TO ACCEPTANCE OF THE PROJECT.

STORM WATER POLLUTION PREVENTION PLAN

CONTRACTOR SHALL EMPLOY MEASURES AND CONSTRUCTION PRACTICES TO PREVENT EROSION AT OR ADJACENT TO THE PROJECT SITE. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR PROVIDING SW3P MEASURES TO COMPLY WITH THE STORM WATER POLLUTION PREVENTION REQUIREMENTS OF LOCAL, STATE, AND FEDERAL REGULATORY AGENCIES INCLUDING, BUT NOT LIMITED TO, THE TEXAS COMMISSION ON ENVIRONMENTAL QUALITY (TCEQ) AND THE UNITED STATES ENVIRONMENTAL PROTECTION AGENCY (EPA). EROSION CONTROL SHALL BEGIN AT THE ONSET OF THE PROJECT (PRIOR TO MOBILIZATION) AND BE MAINTAINED THROUGHOUT THE DURATION OF THE WORK UNTIL FINAL ACCEPTANCE.

ALL PROJECTS FALL UNDER ONE OF THREE CATEGORIES OF SITE DISTURBANCE DEPENDING UPON THE TOTAL AREA BEING DISTURBED. THE FOLLOWING IS ONLY A BRIEF OVERVIEW OF THE REQUIREMENTS. THE CONTRACTOR SHALL REVIEW TCEQ GENERAL PERMIT NUMBER TXR150000 RELATING TO DISCHARGES FROM CONSTRUCTION ACTIVITIES AND IMPLEMENT ALL REQUIRED MEASURES.

✓ LESS THAN 1 ACRE:
IF THE PROJECT DISTURBS LESS THAN 1 ACRE AND IS NOT PART OF A LARGER COMMON PLAN OF DEVELOPMENT, COVERAGE UNDER GENERAL PERMIT (TXR150000) IS NOT REQUIRED.

LIST OF CONTACTS

COMPANY	CONTACT	PHONE NO.
URBAN ENGINEERING	MATT GLAZE	(361) 578-9836
CITY OF PALACIOS	CYNTHIA RALEIGH	(361) 972-3605
RELIANT ENERGY ENTEX	DEAN HOLMES	(361) 573-6251
VERIZON	KATHY WOITENA	(830) 393-3711
AEP CENTRAL POWER & LIGHT		(979) 541-4603
TEXAS ONE CALL		(800) 245-4545

LIST OF ABBREVIATIONS

ABBREVIATIONS	DESCRIPTION
B-B	BACK TO BACK
BC	BACK OF CURB
BM	SURVEY BENCHMARK
CI	CURB INLET
CJ	CONTRACTION JOINT
DCO	DOUBLE CLEAN OUT
EOA	EDGE OF ASPHALT
EOC	EDGE OF CONCRETE
EXIST	EXISTING
EJ	EXPANSION JOINT
FDC	FIRE DEPARTMENT CONNECTION
F-F	FACE TO FACE
FG	FINISHED GRADE
FF	FINISHED FLOOR
FIRE HYD, FH	FIRE HYDRANT
FL	FLOW LINE
FC, FOC	FACE OF CURB
FSR	FOUND STEEL ROD
GI	GRATE INLET
HDPE	HIGH DENSITY POLYETHYLENE
HB	HOSE BIB
LP	LIGHT POLE
L&C	LOCATE & CONNECT
MH	MANHOLE
NG	NATURAL GRADE
NV	NOT VERIFIED
PL	PROPERTY LINE
PP	POWER POLE
PROP	PROPOSED
RCP	REINFORCED CONCRETE PIPE
S.E.T	SLOPED END TREATMENT or SAFETY END TREATMENT
SP	SAMPLE PORT
SAN SWR, SS	SANITARY SEWER
SS CLEANOUT	SANITARY SEWER CLEAN OUT
SS MH	SANITARY SEWER MANHOLE
SSR	SET STEEL ROD
SWR SERVICE, SWR SER	SEWER SERVICE
STM	STORM SEWER
SW	TOP OF SIDEWALK
TBD	TO BE DETERMINED (SEE NOTE 7 OF PRELIMINARY MATTERS)
TC	TOP OF CURB
TEL	TELEPHONE
TG	TOP OF GRATE
TP	TOP OF PAVEMENT
TR	TOP OF RIM
UC	UNDER CONSTRUCTION
UE	UTILITY EASEMENT
WTR	WATER
WV	WATER VALVE
±	INDICATES CONTRACTOR SHALL MATCH EXIST ELEVATION (SEE NOTE 6 OF PRELIMINARY MATTERS)

STATE OF TEXAS
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2/14/23

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- ☐ CONSTRUCTION
- ☐ RECORD DRAWING

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urbanvictoria.com · TREF# F-160

PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

GENERAL NOTES

DATE 02/14/23

JOB NUMBER E23940.00

SHEET

C2

SURVEY LAYOUT & CONTROL

GRAPHIC SCALE IN FEET

0 50 100 150 200

TRUE NORTH

STATE OF TEXAS
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2/14/23

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PROJECT

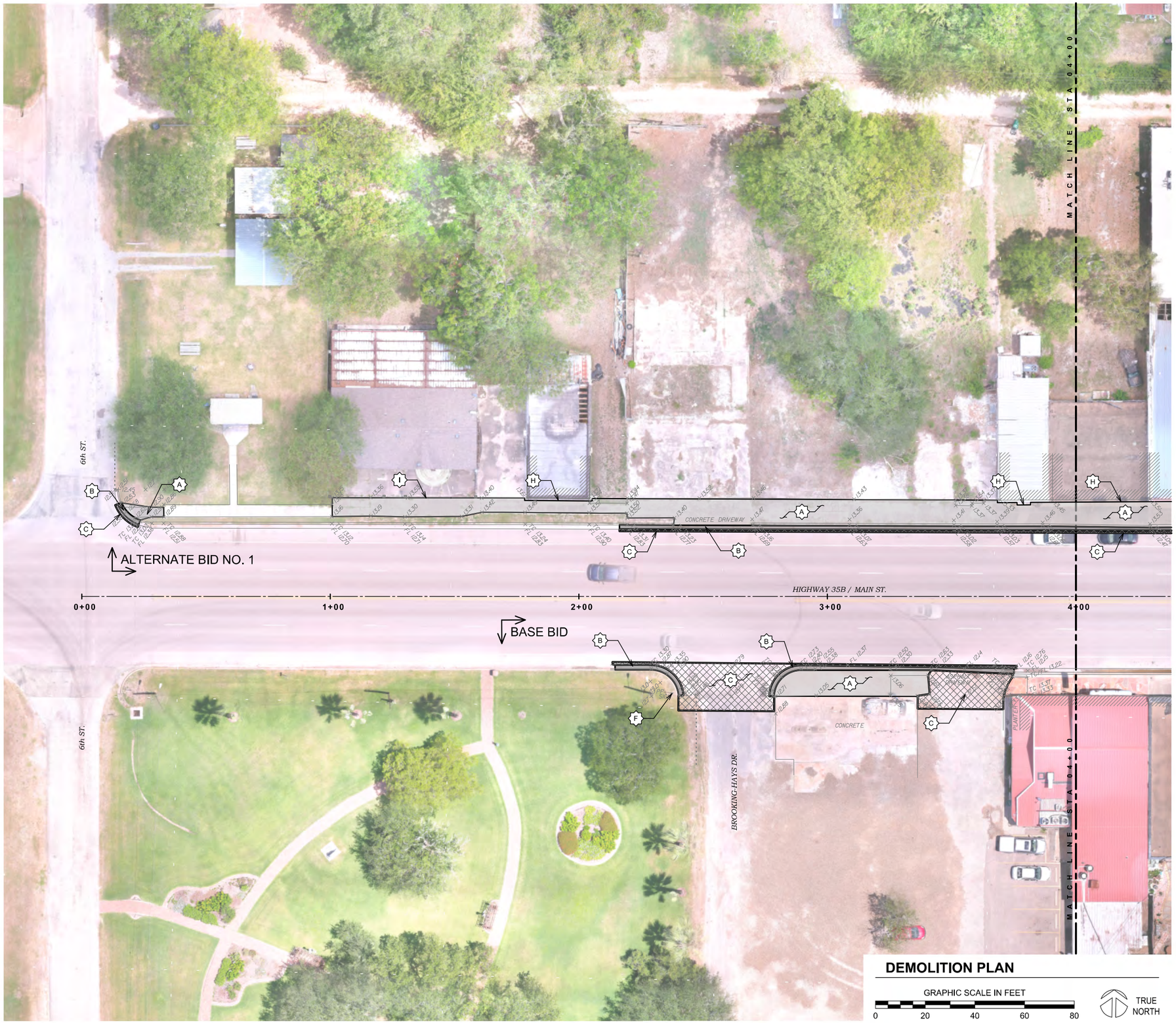
CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT
CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

SURVEY LAYOUT & CONTROL

DATE	02/14/23
JOB NUMBER	E23940.00
SHEET	C3



DEMOLITION PLAN

GRAPHIC SCALE IN FEET
0 20 40 60 80

TRUE NORTH

KEYED NOTES

DEMOLITION

A	EXISTING CONCRETE TO BE REMOVED AND DISPOSED OF BY CONTRACTOR.
B	EXISTING CONCRETE CURB & GUTTER TO BE REMOVED AND DISPOSED OF BY CONTRACTOR.
C	EXISTING ASPHALT TO BE REMOVED AND DISPOSED OF BY CONTRACTOR.
D	ADJUST EXISTING VALVE BOX/METER BOX TO GRADE
E	SAW CUT EXISTING PAVEMENT OR REMOVE AT EXPANSION JOINT.
F	REMOVE EXISTING SIGN/POST.
H	SAW CUT EXISTING CONCRETE 8" - 10" FROM EXISTING BUILDING. PROTECT EXISTING BUILDING FACADE FROM DAMAGE DURING CONSTRUCTION.
I	SAW CUT EXISTING CONCRETE 8" - 10" FROM EXISTING BUILDING. HAND REMOVE AGAINST BUILDING. PROTECT EXISTING FACADE FROM DAMAGE DURING CONSTRUCTION.

STATE OF TEXAS
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ISSUE

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PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT
CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

DEMOLITION PLAN

STATION 0+00 - 4+00

DATE	02/14/23
JOB NUMBER	E23940.00

SHEET

C4.1

DEMOLITION PLAN

GRAPHIC SCALE IN FEET

0 20 40 60 80

TRUE NORTH

KEYED NOTES

DEMOLITION

A	EXISTING CONCRETE TO BE REMOVED AND DISPOSED OF BY CONTRACTOR.
B	EXISTING CONCRETE CURB & GUTTER TO BE REMOVED AND DISPOSED OF BY CONTRACTOR.
C	EXISTING ASPHALT TO BE REMOVED AND DISPOSED OF BY CONTRACTOR.
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PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

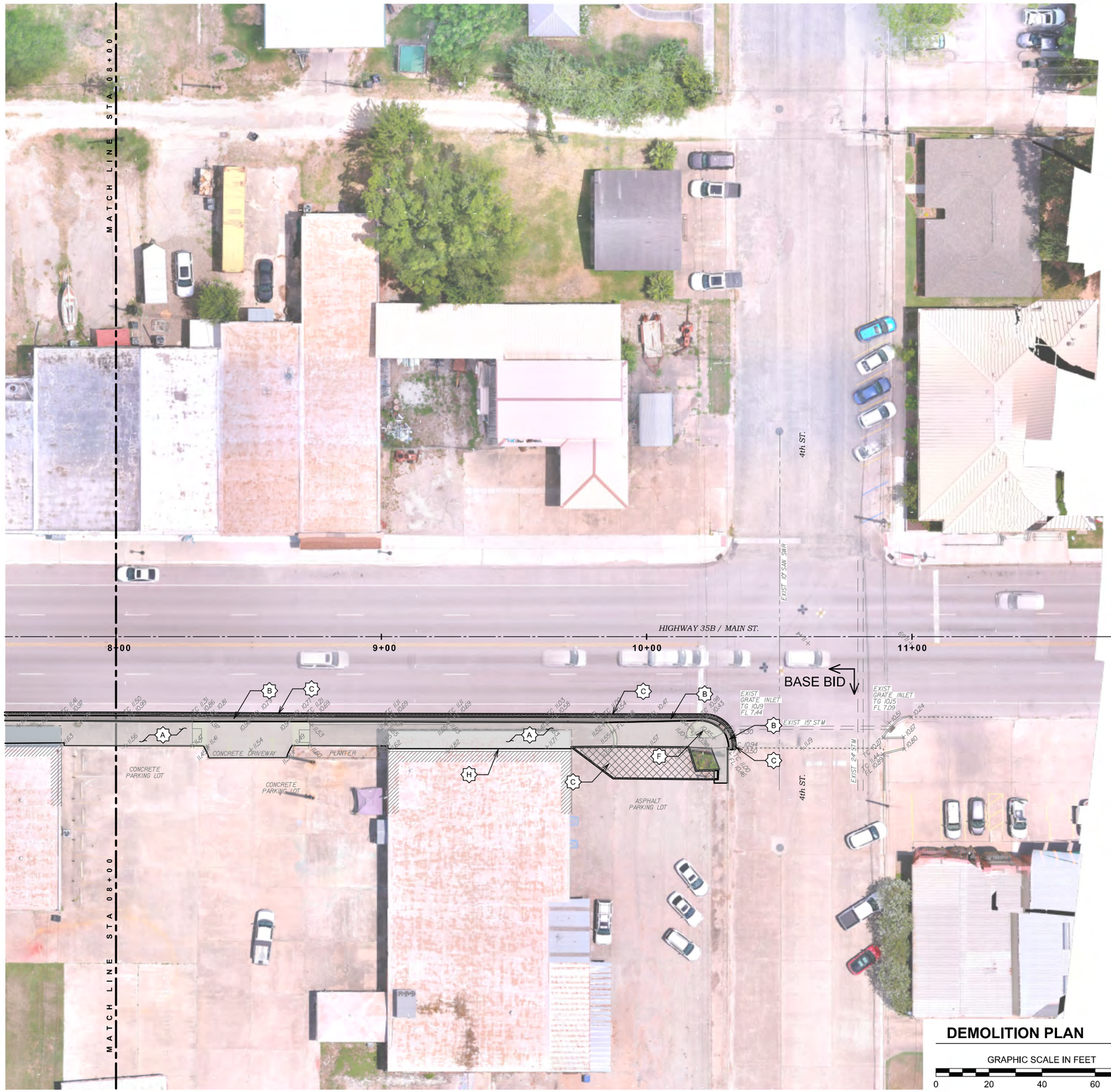
DEMOLITION PLAN

STATION 4+00 - 8+00

DATE	02/14/23
JOB NUMBER	E23940.00

SHEET

C4.2



DEMOLITION PLAN

GRAPHIC SCALE IN FEET

0 20 40 60 80

TRUE NORTH

KEYED NOTES

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PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

DEMOLITION PLAN

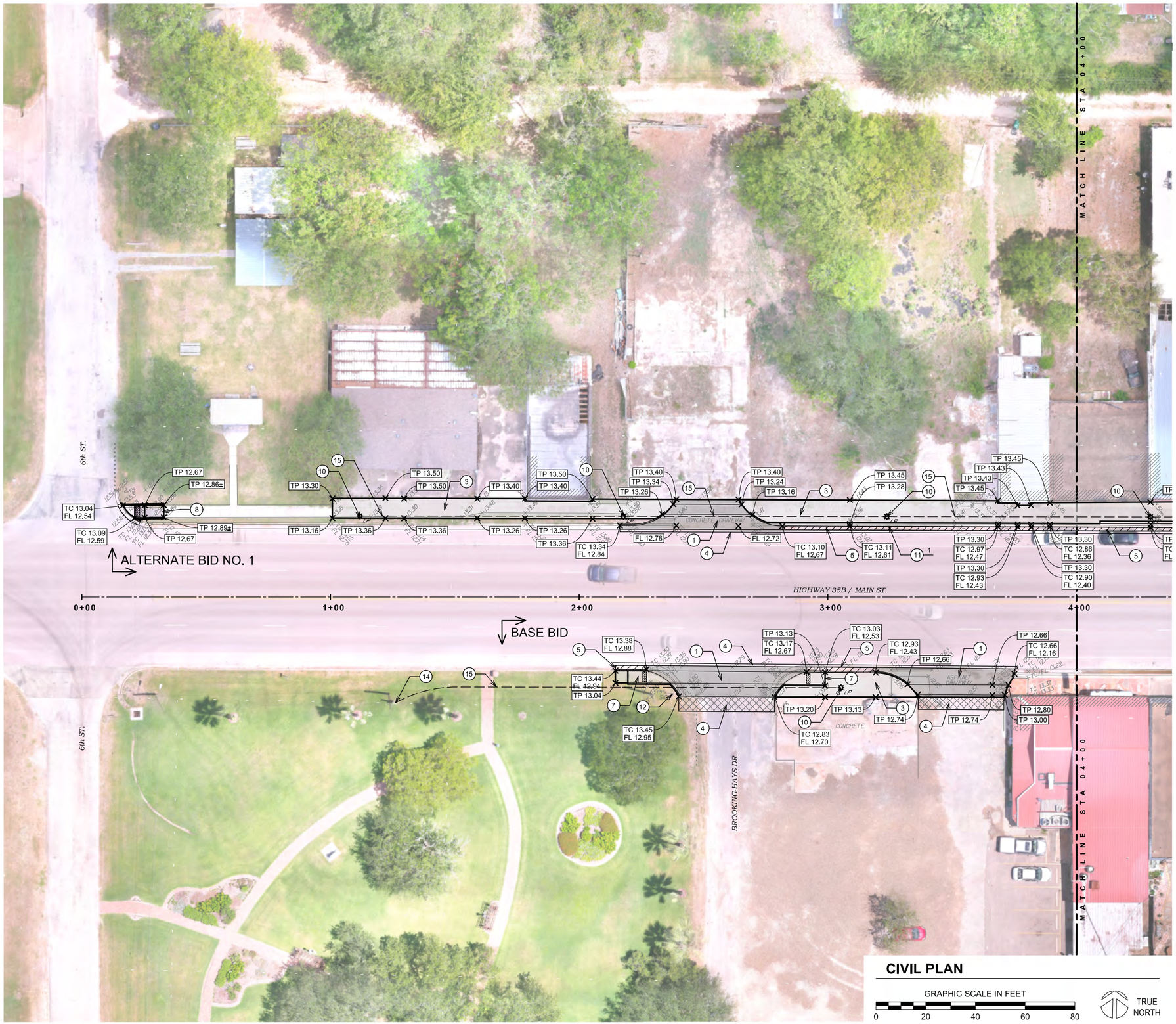
STATION 8+00 - 12+00

DATE 02/14/23

JOB NUMBER E23940.00

SHEET

C4.3



CIVIL PLAN

GRAPHIC SCALE IN FEET

0 20 40 60 80

TRUE NORTH

KEYED NOTES

PAVING	1	CONCRETE DRIVEWAY IN ACCORDANCE WITH THE STANDARD DETAIL. 4/6.1
	2	CONCRETE STREET TRANSITION IN ACCORDANCE WITH THE STANDARD DETAIL. 5/6.1
	3	CONCRETE SIDEWALK IN ACCORDANCE WITH THE STANDARD DETAIL.
	4	ASPHALT PAVEMENT PATCH IN ACCORDANCE WITH STANDARD DETAIL. 1/C6.3
	5	CURB & GUTTER (6") IN ACCORDANCE WITH THE STANDARD DETAIL. 2/C6.2
	6	6" MONOLITHIC CURB IN ACCORDANCE WITH THE STANDARD DETAIL. 1/C6.2
	7	BARRIER FREE CURB RAMP (PARALLEL) IN ACCORDANCE WITH THE STANDARD DETAIL. 1/C6.1
	8	BARRIER FREE CURB RAMP (TYPE 1A) IN ACCORDANCE WITH THE STANDARD DETAIL. 2/C6.1
	9	BARRIER FREE CURB RAMP (TYPE 2B) IN ACCORDANCE WITH THE STANDARD DETAIL. 3/C6.1
	10	STREET LIGHT - SENTRY ELECTRIC SLR-LED RIVERSIDE ON 10' CAST ALUMINUM POLE WITH DECORATIVE BASE
	11	CONCRETE STEPS (TYPE A) IN ACCORDANCE WITH STANDARD DETAIL, 5/C6.2 (NUMBER OF STEPS AS INDICATICTED ON THE PLAN)
	12	INSTALL STREET SIGN IN ACCORDANCE WITH STANDARD DETAIL. 6/C6.1
	13	EXISTING CITY OF PALACIOS METER LOOP & BREAKER BOX
	14	PROPOSED METER POLE, LOAD CENTER & PHOTOCELL IN ACCORDANCE WITH AEP GUIDELINES & CITY CODE.
	15	ELECTRICAL - 2" PVC CONDUIT, WIRES SIZED FOR LOAD, MIN 30" COVER

LEGEND

- 1 — CONCRETE PAVEMENT
- 3 — CONCRETE SIDEWALK
- 4 — ASPHALT PAVEMENT PATCH

STATE OF TEXAS
MATT A. GLAZE
99253
LICENSED
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2/14/23

ISSUE

- [ ] PRELIMINARY
- [ ] REVIEW
- [ ] PERMITTING
- [X] BIDDING
- [ ] CONSTRUCTION
- [ ] RECORD DRAWING

REVISIONS

URBAN engineering
2004 N. Commerce, Victoria, Texas 77901 · 361.578.9836
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PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

CIVIL PLAN

STATION 0+00 - 4+00

DATE	02/14/23
JOB NUMBER	E23940.00
SHEET	C5.1

CIVIL SITE PLAN

GRAPHIC SCALE IN FEET

0 20 40 60 80

TRUE NORTH

KEYED NOTES

PAVING	1	CONCRETE DRIVEWAY IN ACCORDANCE WITH THE STANDARD DETAIL. 4/6.1
	2	CONCRETE STREET TRANSITION IN ACCORDANCE WITH THE STANDARD DETAIL. 5/6.1
	3	CONCRETE SIDEWALK IN ACCORDANCE WITH THE STANDARD DETAIL.
	4	ASPHALT PAVEMENT PATCH IN ACCORDANCE WITH STANDARD DETAIL. 1/C6.3
	5	CURB & GUTTER (6") IN ACCORDANCE WITH THE STANDARD DETAIL. 2/C6.2
	6	6" MONOLITHIC CURB IN ACCORDANCE WITH THE STANDARD DETAIL. 1/C6.2
	7	BARRIER FREE CURB RAMP (PARALLEL) IN ACCORDANCE WITH THE STANDARD DETAIL. 1/C6.1
	8	BARRIER FREE CURB RAMP (TYPE 1A) IN ACCORDANCE WITH THE STANDARD DETAIL. 2/C6.1
	9	BARRIER FREE CURB RAMP (TYPE 2B) IN ACCORDANCE WITH THE STANDARD DETAIL. 3/C6.1
	10	STREET LIGHT - SENTRY ELECTRIC SLR-LED RIVERSIDE ON 10' CAST ALUMINUM POLE WITH DECORATIVE BASE
	11	CONCRETE STEPS (TYPE A) IN ACCORDANCE WITH STANDARD DETAIL, 5/C6.2 (NUMBER OF STEPS AS INDICATICTED ON THE PLAN)
	12	INSTALL STREET SIGN IN ACCORDANCE WITH STANDARD DETAIL. 6/C6.1
	13	EXISTING CITY OF PALACIOS METER LOOP & BREAKER BOX
	14	PROPOSED METER POLE, LOAD CENTER & PHOTOCELL IN ACCORDANCE WITH AEP GUIDELINES & CITY CODE.
	15	ELECTRICAL - 2" PVC CONDUIT, WIRES SIZED FOR LOAD, MIN 30" COVER

LEGEND

1	CONCRETE PAVEMENT
3	CONCRETE SIDEWALK
4	ASPHALT PAVEMENT PATCH

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PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

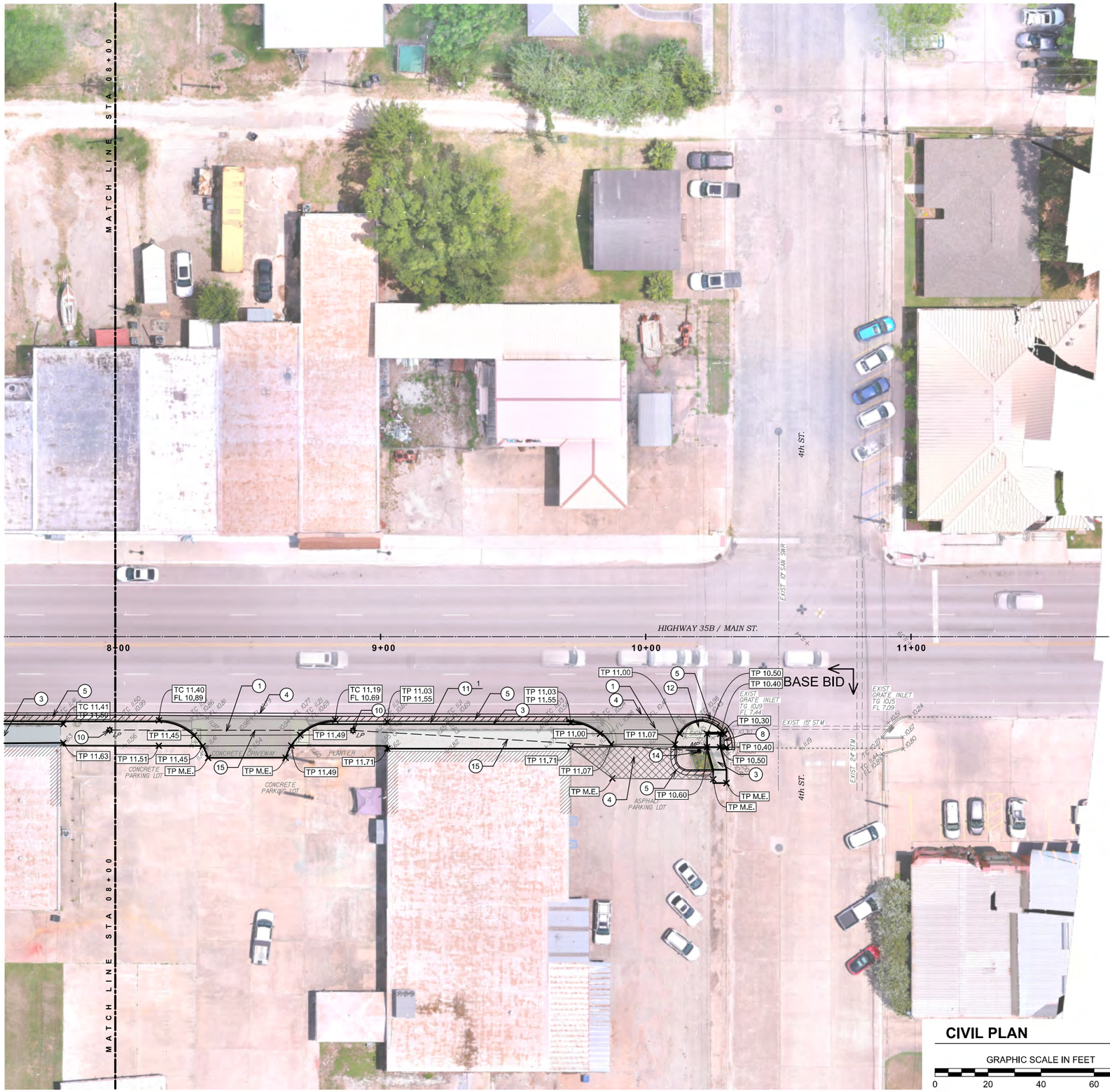
MAIN STREET
PALACIOS, TEXAS

TITLE

CIVIL PLAN

STATION 4+00 - 8+00

DATE	02/14/23
JOB NUMBER	E23940.00
SHEET	C5.2



CIVIL PLAN

GRAPHIC SCALE IN FEET

0 20 40 60 80

TRUE NORTH

KEYED NOTES

PAVING	1	CONCRETE DRIVEWAY IN ACCORDANCE WITH THE STANDARD DETAIL. 4/6.1
	2	CONCRETE STREET TRANSITION IN ACCORDANCE WITH THE STANDARD DETAIL. 5/6.1
	3	CONCRETE SIDEWALK IN ACCORDANCE WITH THE STANDARD DETAIL.
	4	ASPHALT PAVEMENT PATCH IN ACCORDANCE WITH STANDARD DETAIL. 1/C6.3
	5	CURB & GUTTER (6") IN ACCORDANCE WITH THE STANDARD DETAIL. 2/C6.2
	6	6" MONOLITHIC CURB IN ACCORDANCE WITH THE STANDARD DETAIL. 1/C6.2
	7	BARRIER FREE CURB RAMP (PARALLEL) IN ACCORDANCE WITH THE STANDARD DETAIL. 1/C6.1
	8	BARRIER FREE CURB RAMP (TYPE 1A) IN ACCORDANCE WITH THE STANDARD DETAIL. 2/C6.1
	9	BARRIER FREE CURB RAMP (TYPE 2B) IN ACCORDANCE WITH THE STANDARD DETAIL. 3/C6.1
	10	STREET LIGHT - SENTRY ELECTRIC SLR-LED RIVERSIDE ON 10' CAST ALUMINUM POLE WITH DECORATIVE BASE
	11	CONCRETE STEPS (TYPE A) IN ACCORDANCE WITH STANDARD DETAIL, 5/C6.2 (NUMBER OF STEPS AS INDICATICTED ON THE PLAN)
	12	INSTALL STREET SIGN IN ACCORDANCE WITH STANDARD DETAIL. 6/C6.1
	13	EXISTING CITY OF PALACIOS METER LOOP & BREAKER BOX
	14	PROPOSED METER POLE, LOAD CENTER & PHOTOCELL IN ACCORDANCE WITH AEP GUIDELINES & CITY CODE.
	15	ELECTRICAL - 2" PVC CONDUIT, WIRES SIZED FOR LOAD, MIN 30" COVER

LEGEND

- CONCRETE PAVEMENT
- CONCRETE SIDEWALK
- ASPHALT PAVEMENT PATCH

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PROJECT
CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT
CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE
CIVIL PLAN

STATION 8+00 - 12+00

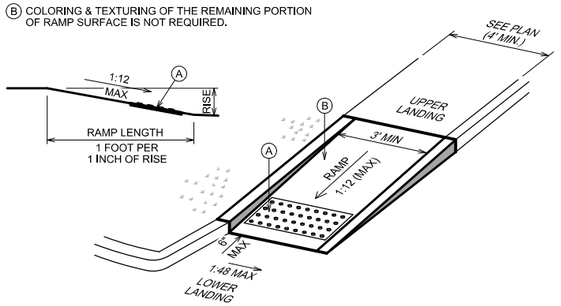
DATE	02/14/23
JOB NUMBER	E23940.00
SHEET	C5.3

GENERAL NOTES

1. EXPANSION JOINTS ARE REQUIRED AT THE TOP, BOTTOM, AND EDGES OF RAMPS.
2. MAXIMUM CROSS SLOPE FOR THE LANDINGS, RAMP AND ACCESSIBLE ROUTE SHALL BE 1:48.

(A) TRUNCATED DOME DETECTABLE WARNINGS SHALL BE CAST IN PLACE PRODUCT BY ADA , INC. OR APPROVED EQUAL. www.adatile.com/castinplace.php.

(B) COLORING & TEXTURING OF THE REMAINING PORTION OF RAMP SURFACE IS NOT REQUIRED.



1 DIRECTIONAL RAMP

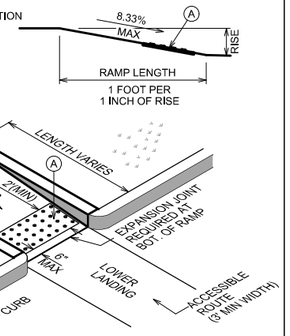
(PERPENDICULAR/PARALLEL RAMP)

GENERAL NOTES

1. MAXIMUM LONGITUDINAL SLOPE FOR THE UPPER AND LOWER LANDING SHALL BE 1:20 (5%).
2. MAXIMUM LONGITUDINAL SLOPE FOR THE RAMP SHALL BE 1:12 (8.33%).
3. MAXIMUM LONGITUDINAL SLOPE FOR THE ACCESSIBLE ROUTE SHALL BE 1:20 (5%).
4. MAXIMUM CROSS SLOPE FOR THE LANDINGS, RAMP AND ACCESSIBLE ROUTE SHALL BE 1:50 (2%).

(A) TRUNCATED DOME DETECTABLE WARNINGS SHALL BE CAST IN PLACE PRODUCT BY ADA , INC. OR APPROVED EQUAL. www.adatile.com/castinplace.php.

(B) COLORING & TEXTURING OF THE REMAINING PORTION OF RAMP SURFACE IS NOT REQUIRED.



2 BARRIER FREE CURB RAMP

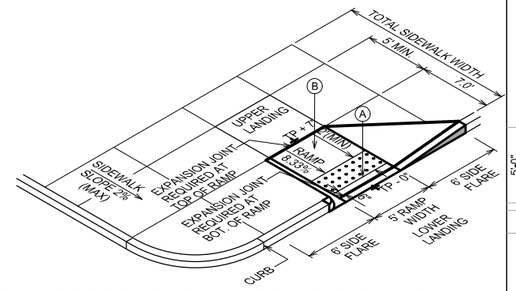
(TYPE 1A - FOR USE WITHIN PUBLIC RIGHT-OF-WAY)

GENERAL NOTES

1. MAXIMUM LONGITUDINAL SLOPE FOR THE UPPER AND LOWER LANDING SHALL BE 1:20 (5%).
2. MAXIMUM LONGITUDINAL SLOPE FOR THE RAMP SHALL BE 1:12 (8.33%).
3. MAXIMUM LONGITUDINAL SLOPE FOR THE ACCESSIBLE ROUTE SHALL BE 1:20 (5%).
4. MAXIMUM CROSS SLOPE FOR THE LANDINGS, RAMP AND ACCESSIBLE ROUTE SHALL BE 1:50 (2%).

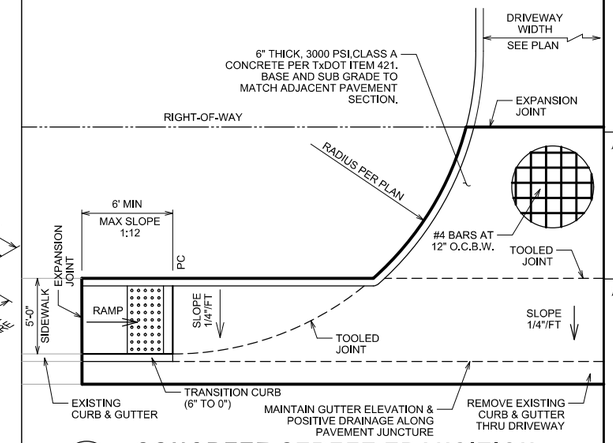
(A) TRUNCATED DOME DETECTABLE WARNINGS SHALL BE CAST IN PLACE PRODUCT BY ADA , INC. OR APPROVED EQUAL. www.adatile.com/castinplace.php.

(B) COLORING & TEXTURING OF THE REMAINING PORTION OF RAMP SURFACE IS NOT REQUIRED.



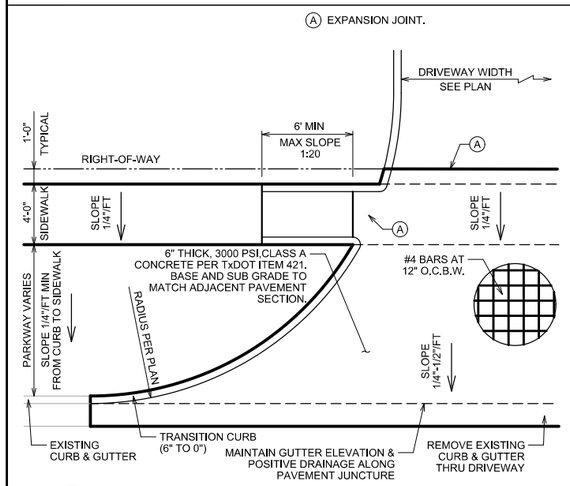
3 PERPENDICULAR CURB RAMP

(TYPE 2B - COMBINATION RAMP)



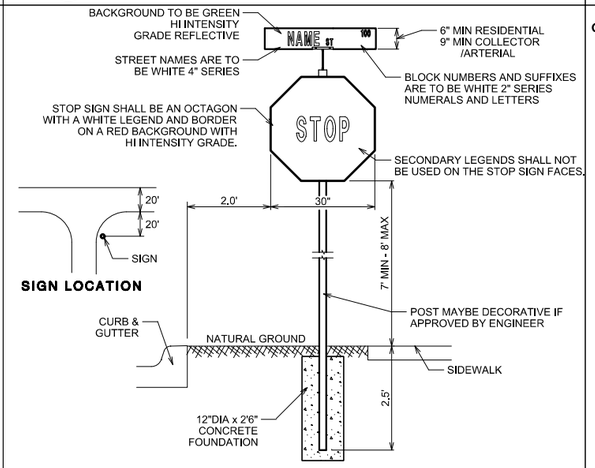
4 CONCRETE STREET TRANSITION

(WITH SIDEWALK ADJACENT TO STREET)



5 COMMERCIAL DRIVEWAY RAMP

(WITH SIDEWALK REMOTE FROM STREET)

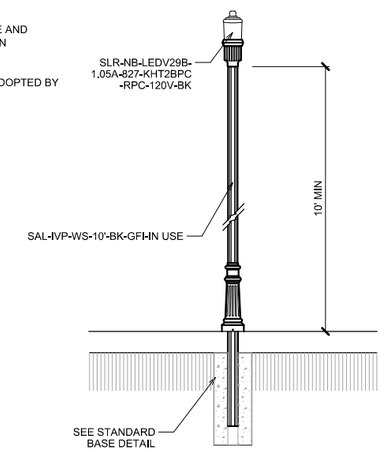


6 TRAFFIC SIGN

(CONVENTIONAL ROADS 30" SIGN)

GENERAL NOTES

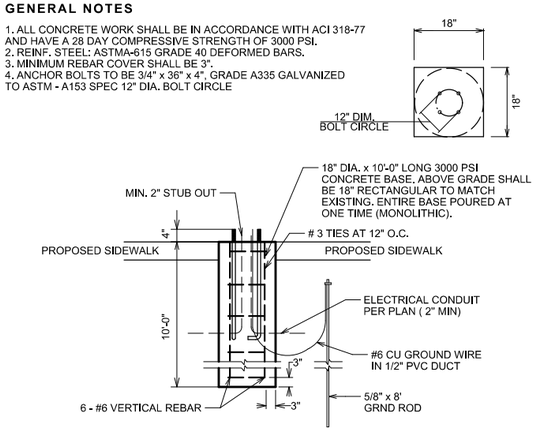
1. PROVIDE SAMPLE LIGHT POLE AND FIXTURE PRIOR TO INSTALLATION
2. ALL ELECTRICAL SHALL BE IN ACCORDANCE WITH THE NATIONAL ELECTRIC CODE AS ADOPTED BY THE CITY.



7 DECORATIVE LIGHT POLE

GENERAL NOTES

1. ALL CONCRETE WORK SHALL BE IN ACCORDANCE WITH ACI 318-77 AND HAVE A 28 DAY COMPRESSIVE STRENGTH OF 3000 PSI.
2. REINF. STEEL: ASTMA-615 GRADE 40 DEFORMED BARS.
3. MINIMUM REBAR COVER SHALL BE 3".
4. ANCHOR BOLTS TO BE 3/4" x 36" x 4". GRADE A335 GALVANIZED TO ASTM - A153 SPEC 12" DIA. BOLT CIRCLE



8 LIGHT STANDARD BASE

STATE OF TEXAS
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PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

DETAILS

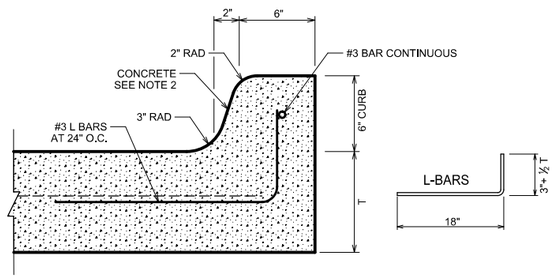
DATE	02/14/23
JOB NUMBER	E23940.00

SHEET

C6.1

GENERAL NOTES

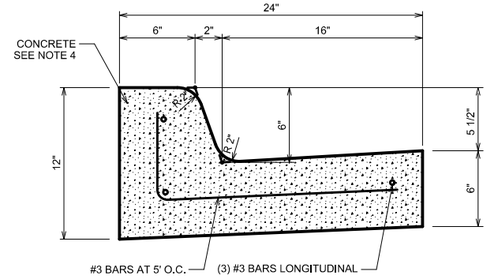
1. CURB SHALL BE PLACED MONOLITHICALLY WITH THE ADJACENT CONCRETE.
2. CONCRETE SPECIFICATIONS SHALL BE THE SAME AS THOSE FOR THE PAVEMENT.
3. THE COMPLETED CONCRETE CURB SHALL NOT VARY FROM THE SHAPE, DIMENSION AND CONTOUR SHOWN ON THE DETAIL BY MORE THAN 1/2" AT ANY POINT ALONG THE CURB. NON-CONFORMING CURB SHALL BE REMOVED AND REPLACED BY A SAW CUTTING 6" IN FRONT OF THE DEFECTIVE CURB, THEN REMOVING AND REPLACING THE ENTIRE DEPTH OF CURB AND PAVEMENT IN THE DEFECTIVE AREA.
4. EXPANSION JOINTS IN THE PAVEMENT SHALL EXTEND THROUGH ADJACENT CURB.



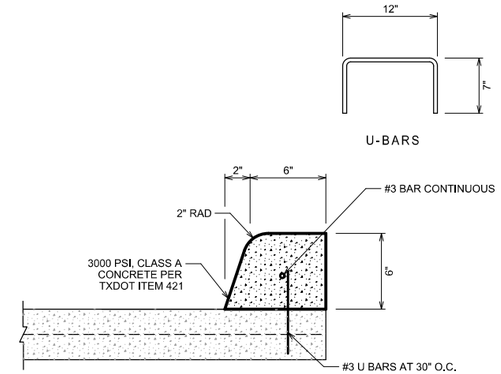
1 6" MONOLITHIC CURB

GENERAL NOTES

1. EXPANSION JOINTS SHALL BE LOCATED AT 40' INTERVALS. EXPANSION MATERIAL SHALL BE 3/4" BITUMINOUS FELT. USE (3) 5/8" DOWELS WITH SLEEVES.
2. CONTRACTION JOINTS SHALLED BE SCORED AT 10' INTERVALS.
3. TRANSVERSE BARS MAY BE OMITTED IF CURB & GUTTER IS MACHINE LAID.
4. CONCRETE SHALL MEET THE REQUIREMENTS OF TXDOT (2014) ITEM 421 –HYDRAULIC CEMENT CONCRETE, CLASS A, WITH THE FOLLOWING MODIFICATIONS: MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3,500 PSI; SLUMP SHALL RANGE FROM 4 TO 6-1/2 INCHES AT THE POINT OF PLACEMENT.



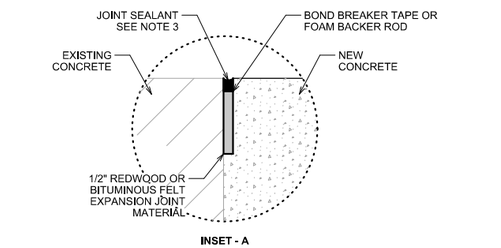
2 CONCRETE CURB & GUTTER



3 6" CONCRETE CURB

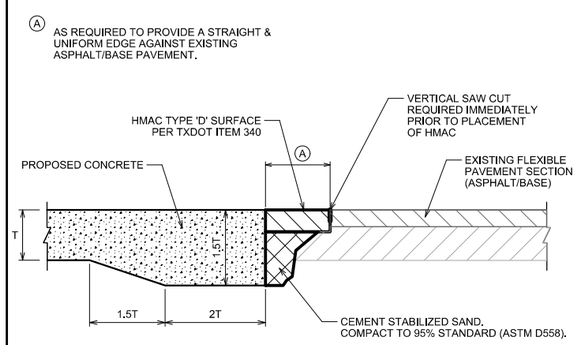
GENERAL NOTES

1. CONSTRUCTION JOINTS ARE REQUIRED AT ANY LOCATION WHERE NEW CONCRETE IS PLACED AGAINST EXISTING CONCRETE PAVEMENT OR STRUCTURES. THIS INCLUDES BUILDINGS, PARKING LOTS, SIDEWALKS, PATIOS, CURBS, INLETS, MANHOLES, HEADWALLS, LIGHT POLES, ETC. AND APPLIES TO CONCRETE THAT WAS PLACED EARLIER DURING THE PROJECT AND CONCRETE THAT WAS EXISTING PRIOR TO THE START OF THIS PROJECT.
2. CONSTRUCTION JOINTS WITHIN NEW PAVEMENT SHOULD BE PLANNED TO COINCIDE WITH EXPANSION JOINT LOCATIONS. WHEN THIS IS NOT FEASIBLE, THEN A KEYWAY STYLE CONSTRUCTION JOINT SHALL BE UTILIZED. THE LOCATION OF THE KEYWAY CONSTRUCTION JOINT SHALL COINCIDE WITH THE LOCATION OF A PLANNED CONTRACTION JOINT.
3. CONSTRUCTION JOINT SHALL BE SEALED WITH SONNEBORN BUILDING PRODUCT: SONOLASTIC SL-1: NON-PRIMING, ONE-PART, SELF LEVELING POLYURETHANE SEALANT OR APPROVED EQUAL. JOINT CLEANING AND PREPARATION SHALL BE IN STRICT ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.



GENERAL NOTES

1. CEMENT STABILIZED SAND SHALL CONTAIN A MINIMUM OF 1 1/2 SACKS OF CEMENT PER CUBIC YARD. THE SAND SHALL BE SP, SW OR SM PER USC (ASTM D2487) AND SHALL HAVE THE FOLLOWING PROPERTIES: LESS THAN 15% PASSING #200 SIEVE (PER ASTM C136); LESS THAN 0.5% CLAY LUMPS; PLASTICITY LESS THAN 4; LIQUID LIMIT LESS THAN 25.

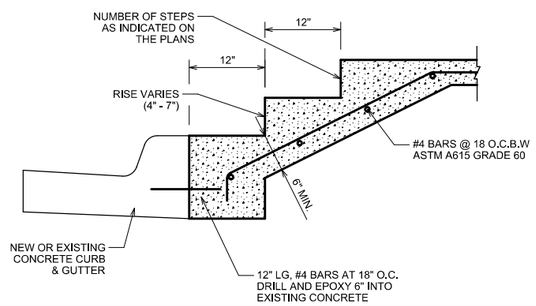


4 PAVEMENT PATCH

(PROPOSED CONCRETE TO EXISTING FLEXIBLE PAVEMENT)

GENERAL NOTES

1. CONCRETE SHALL MEET THE REQUIREMENTS OF TXDOT (2004) ITEM 421 - HYDRAULIC CEMENT CONCRETE, CLASS A (3,000 PSI), WITH THE FOLLOWING MODIFICATIONS: SLUMP SHALL RANGE FROM 4 INCHES TO 6-1/2 INCHES AT THE POINT OF PLACEMENT.
2. REINFORCING STEEL SHALL BE ASTM A615, GRADE 60.

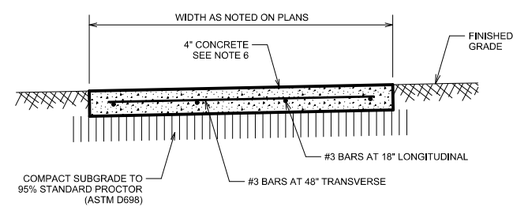


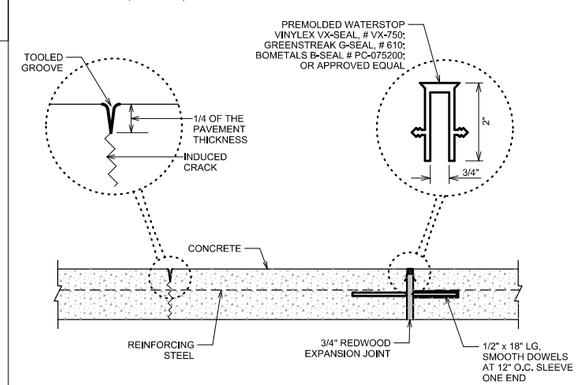
5 CONCRETE STEPS

(TYPE A)

GENERAL NOTES

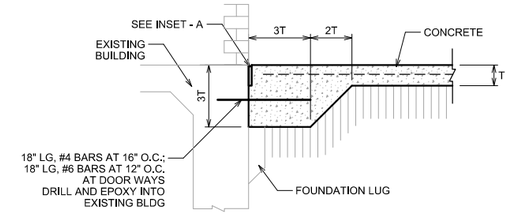
1. REINFORCING STEEL SHALL BE ASTM A615, GRADE 40.
2. EXPANSION JOINTS SHALL BE LOCATED AT DRIVEWAYS AND OTHER FIXED OBJECTS, AND AT 45' MAXIMUM INTERVALS ALONG THE SIDEWALK OR AS INDICATED ON PLANS.
3. EXPANSION JOINT SHALL BE SEALED SONNEBORN BUILDING PRODUCT: SONOLASTIC SL-1: NON-PRIMING, ONE-PART, SELF LEVELING POLYURETHANE SEALANT OR APPROVED EQUAL. JOINT CLEANING AND PREPARATION SHALL BE IN STRICT ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
4. TOOLED CONTRACTION JOINTS SHALL BE LOCATED AT REGULAR INTERVALS EQUAL TO THE WIDTH OF THE SIDEWALK OR AS INDICATED ON PLANS. JOINTS SHALL BE SPACED TO SO THAT THE RESULTING PANELS ARE SQUARE. IN NO CASE SHOULD THE LENGTH OF A PANEL EXCEED 1.5 TIMES THE WIDTH.
5. FINISH EXPOSED EDGES WITH 1/4" RADIUS.
6. CONCRETE SHALL MEET THE REQUIREMENTS OF TXDOT (2014) ITEM 421 –HYDRAULIC CEMENT CONCRETE, CLASS A (3,000 PSI), WITH THE FOLLOWING MODIFICATIONS: SLUMP SHALL RANGE FROM 4 INCHES TO 6-1/2 INCHES AT THE POINT OF PLACEMENT.
7. MAXIMUM LONGITUDINAL SLOPE FOR ANY SIDEWALK SHALL BE 1:20 (5.0%)
8. MAXIMUM TRANSVERSE SLOPE FOR ANY SIDEWALK SHALL BE 1:50 (2.0%)



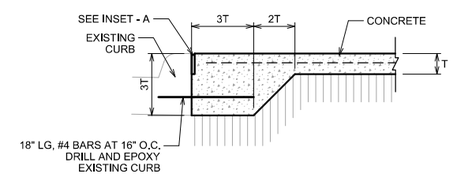


EXPANSION & TOOLED CONTRACTION JOINTS

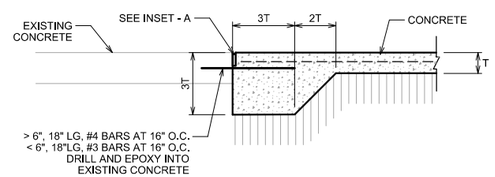
6 CONCRETE SIDEWALK



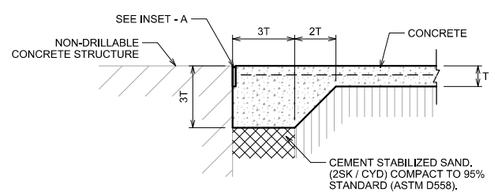
CONCRETE ADJACENT TO BUILDING



CONCRETE ADJACENT TO CONCRETE CURB



CONCRETE ADJACENT TO EXISTING CONCRETE



CONCRETE ADJACENT TO NON-DRILLABLE STRUCTURE

7 CONSTRUCTION JOINTS

STATE OF TEXAS
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2/14/23

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PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

DETAILS

DATE	02/14/23
JOB NUMBER	E23940.00

SHEET

C6.2

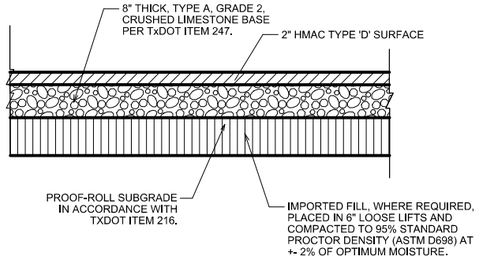
GENERAL NOTES

1. SAWCUT ALL EXISING PAVEMENT JOINTS PRIOR TO REMOVING EXISITNG PAVEMENT.

2. ONCE FINAL SUBGRADE ELEVATION HAS BEEN ACHIEVED, THE EXPOSED SUBGRADE SHALL BE PROOF-ROLLED IN ACCORDANCE WITH TXDOT ITEM 216 TO DETECT WEAK ZONES. WEAK AREAS DETECTED DURING THE PROOF-ROLLING PROCESS SHALL BE REMOVED AND REPLACE WITH SOILS MEETING THE REQUIREMENTS FOR IMPORTED FILL.

3. BASE MATERIAL SHALL BE CRUSHED LIMESTONE OR CRUSHED CONCRETE, MEETING THE REQUIREMENTS OF TXDOT (2004) ITEM 247, TYPE A OR TYPE D, GRADE 1. THE BASE MATERIAL SHALL BE COMPACTED TO 95% MODIFIED PROCTOR DENSITY (ASTM D-1557) AT A MOISTURE CONTENT WITHIN 2% OF OPTIMUM.

6. HOT MIX ASPHALTIC CONCRETE SURFACE COURSE SHALL BE PLANT MIXED, HOT LAID, TYPE D, FINE GRADED SURFACE COURSE, MEETING THE REQUIREMENTS OF TXDOT (2014), ITEM 340.

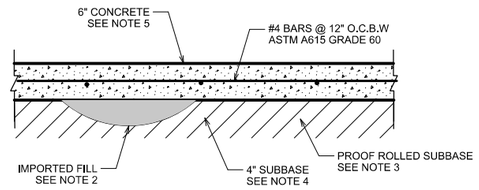


1 ASPHALT PAVEMENT PATCH

(8" CRUSHED LIMESTONE ON PROOF ROLLED SUBGRADE)

GENERAL NOTES

1. STRIP AND REMOVE ALL VEGETATION, LOOSE TOPSOIL, TREES AND ROOTS WITHIN THE CONSTRUCTION AREA.
2. IMPORTED FILL MATERIALS, WHEN REQUIRED TO ACHIEVE FINAL SUBGRADE ELEVATION, SHALL BE CHOSEN THAT EXHIBIT SIMILAR CLASSIFICATION AND PHYSICAL PROPERTIES AS THE IN SITU SOILS. SAMPLES OF THE FILL MATERIAL SHALL BE MADE AVAILABLE TO THE OWNER'S TESTING LABORATORY 7 DAYS PRIOR TO PLACEMENT. SUBGRADE FILL, WHERE REQUIRED, SHALL BE PLACED IN 6" LOOSE LIFTS AND COMPACTED TO 95% STANDARD PROCTOR DENSITY (ASTM D698) AT +- 2% OF OPTIMUM MOISTURE.
3. ONCE FINAL SUBGRADE ELEVATION HAS BEEN ACHIEVED, THE EXPOSED SUBGRADE SHALL BE PROOF-ROLLED IN ACCORDANCE WITH TXDOT ITEM 216 TO DETECT WEAK ZONES. WEAK AREAS DETECTED DURING THE PROOF-ROLLING PROCESS, AS WELL AS ZONES CONTAINING DEBRIS AND/OR ORGANICS SHALL BE REMOVED AND REPLACED WITH SOILS MEETING THE REQUIREMENTS FOR IMPORTED FILL.
4. SUBBASE SHALL CONSIST OF TYPE B, GRADE 3, PROCESSED GRAVEL BASE, PER TXDOT (2004) ITEM 247, COMPACT TO 95% STANDARD PROCTOR DENSITY (ASTM D698) AT A MOISTURE CONTENT BETWEEN -1% & +3% OF OPTIMUM. PLASTICITY INDEX OF BASE MATERIAL MAY NOT EXCEED 16.
5. CONCRETE SHALL MEET THE REQUIREMENTS OF TXDOT (2004) ITEM 421 –HYDRAULIC CEMENT CONCRETE, CLASS A, WITH THE FOLLOWING MODIFICATIONS; MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3,500 PSI; SLUMP SHALL RANGE FROM 4 INCHES TO 6-1/2 INCHES AT THE POINT OF PLACEMENT.



2 CONCRETE PAVEMENT

STATE OF TEXAS
MATT A. GLAZE
99253
LICENSED
PROFESSIONAL ENGINEER
2/14/23

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PROJECT

CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT
CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE

DETAILS

DATE	02/14/23
JOB NUMBER	E23940.00

SHEET

C6.3

TEXAS HISTORICAL COMMISSION

STREETSCAPE GUIDELINES FOR HISTORIC COMMERCIAL DISTRICTS

When street and sidewalk improvement projects receive federal funding and oversight, federal agencies or communities must consult with the Texas Historical Commission (THC) in accordance with Section 106 of the National Historic Preservation Act. Examples of commonly funded federal programs for streetscape projects in Texas include the Texas Capital Fund (U.S. Department of Housing and Urban Development funding administered by the Texas Department of Agriculture) and the Statewide Transportation Enhancement Program (Federal Highway Administration funding administered by the Texas Department of Transportation). Section 106 requires consideration of the potential effects of a project on historic properties. If your project area is identified as including historic properties in consultation with the THC**, continued close coordination with the THC regarding the design of your project will be a key element for success.** Successful projects tailor design solutions to preserve the history and character of an individual community. The THC's goal in guiding a project is to preserve significant elements and introduce new features that are compatible with the historic character of a community, not to freeze the appearance of a streetscape in time or restore it to a particular period.

As each community in Texas is unique, the design of each project should be tailored to respond to the history and needs of that specific community. In addition, community input and the creativity of your design professional are important components of the design process, and in many cases more than one option can offer a preservation-oriented solution. The THC, therefore, does not set strict rules, but instead assists design decisions, basing its review on the Secretary of the Interior's *Standards for Rehabilitation*. These 10 standards, developed by the National Park Service for projects that require sensitive treatment of historic properties while allowing for their modification to meet continued or changed uses, are as follows (italicized sections represent the THC's interpretation of these standards for streetscape projects):

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment. *Support the use and preservation of the area's historic buildings and streetscape features. Allow the historic setting, human activity, individual businesses, and special events to provide the cultural stimulus and revitalization.*
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided. *Identify historic streetscape features in your project area and plan for their preservation.*
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken. *Study the historic appearance of your streetscape and avoid adding elements from other communities that were not historically present in yours.*
4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved. *Understand the period within which important events and growth occurred in your historic downtown. Respect the character of buildings and elements added during that period of significance.*
5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved. *Protect significant historic streetscape features and incorporate them into your project. Protect adjacent historic buildings from damage during construction.*
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the

massing, size, scale, and architectural features to protect the historic integrity of the property and its environment. *Design new elements, such as ramps for accessibility, to be in keeping with the historic character of the streetscape but distinguishable as modern additions. Respect the existing character of the project area. Avoid the urge to overdesign and recognize that new features should be secondary, supporting elements.*
10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

Guidelines for the design and treatment of sidewalks and streets, accessibility, corner extensions, trees and vegetation, historic canopies and awnings, street furniture, street lights, artwork and decoration, and historic buildings are listed below to assist you in the application of the *Standards* to streetscape projects. THC staff can provide additional assistance regarding how to interpret this guidance in the context of your project and historic commercial district. Please do not hesitate to contact us with questions.

The THC encourages you to consult and incorporate these guidelines as early as possible, ideally before public meetings are held to discuss the scope and nature of the streetscape project. Material may be submitted to the THC for preliminary review early in the design process to ensure that the proposed work is in keeping with the *Standards*. To complete the Section 106 process, construction documents nearing finalization must be submitted for THC review. If the proposed work meets the *Standards*, a determination of "no adverse effect to historic properties" will conclude the review process and the project may proceed without further consultation; however, any changes made following this determination must be submitted for review. Please see our website at http://www.thc.state.tx.us/crm/crmdefault.shtml for additional information on the Section 106 process and what to submit for a project review. The project must be coordinated separately with other state agencies with regulatory authority.

SIDEWALKS AND STREETS

- Identify historic features of the sidewalks and streets, such as high or stepped curbs, stone or brick curbs, stamped concrete in the sidewalks, historic manhole covers, mosaic tile street names, tiled store entries, trolley tracks, and brick streets. When these elements contribute to the distinctive character of your historic downtown, preserve them in place whenever possible.
- Repair damaged brick streets if necessary and perform selective, in-kind replacement with matching brick for areas damaged beyond repair. Likewise, repair historic sidewalks or consider replacing them with like material if damaged beyond repair.
- If sidewalk replacement is necessary, the preferred option is to use either the same material as currently exists or a material used in the location historically, based on adequate documentation. Unless documentation shows historic brick sidewalks, accepted preservation practice discourages the introduction of brick or modern pavers to sidewalks as it can create a false sense of historical development of the community. However, the use of removable concrete panels or pavers for ease of access to in-ground utility boxes or channels is acceptable, when the color of the pavers is similar to or matching the existing sidewalk. Similarly, pavers may be used to distinguish pedestrian zones in areas where there are multiple curb cuts or no curb separating the sidewalk from the street. Avoid pavers that are brightly colored, multicolored, placed in decorative patterns, or inscribed with donors' names, because they can be distracting from the historic character of a commercial district.
- If you wish to introduce variety to concrete sidewalks, vary the texture by the use of a simple trowel or broom finish. Elaborate paving patterns, multiple colors, and different materials distract users and are generally not compatible with the character of historic streetscapes. However, where a variety of historic materials contribute to the varied character of the historic streetscape, these materials should be preserved.

ACCESSIBILITY

- The THC supports efforts to make historic commercial districts accessible by bringing sidewalks into compliance with Texas Accessibility Standards (TAS) and the Americans with Disabilities Act (ADA) Accessibility Guidelines. However, an effort should be made to balance new features, such as ramps and handrails, with the historic character of the commercial district.
- Retain historic high curbs with the addition of new steps and accessible ramps in a compatible material. Where

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PROJECT
CITY OF PALACIOS - DOWNTOWN REVITALIZATION PROJECT CDM21-0045

MAIN STREET
PALACIOS, TEXAS

TITLE
STREETSCAPE GUIDELINES

DATE 02/14/23
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possible, install these features on side streets to better preserve the main street's historic appearance.

- Railings may be necessary at ramps or along sidewalks to meet current accessibility and safety requirements. Select railings that are simple in design and as unobtrusive as possible so as not to block views or distract from the historic storefronts.

CORNER EXTENSIONS

- Corner extensions (expansion of the sidewalk into the parking lane at street corners, also known as bump-outs or aprons) are sometimes necessary when sidewalks are too narrow or have too high an elevation for other solutions, for the control of vehicle traffic and parking, or for the provision of pedestrian safety. However, when not carefully considered, these new elements can dramatically change the character of a block or district.
- When corner extensions are planned, either minimize their size as much as possible and use materials that are compatible in color and texture with the historic sidewalk, or allow these spaces to be the foci for introducing new elements such as light poles, planters, trees, and benches.

TREES AND VEGETATION

- Historically, trees and vegetation were primarily planted on residential streets and around civic buildings like courthouses, post offices, and libraries. In commercial districts, canopies and awnings (see below) were the most common method of providing shade to sidewalks. Historic documentation, such as photographs of the downtown area, should inform your decisions about whether to plant trees in your commercial district.
- New plantings reduce can visibility to storefronts and signs, block future restoration of canopies, and obscure street lighting. When introducing trees, plant them on side streets with no or limited storefronts, in corner extensions, or in specific spaces such as pocket parks, vacant lots, parking lots, and blocks with non-historic buildings. Vegetation should be low in profile so as not to block historic features of buildings and districts.
- Take into account necessary periodic maintenance of proposed trees and other plantings, their likelihood of survival in a heavily paved area, and potential future damage to paving from root growth.

HISTORIC CANOPIES AND AWNINGS

Historically, canopies (projecting roof structures) and awnings (lightweight shade structures, often fabric stretched over a frame) sheltered entrances to businesses and shaded sidewalks in commercial districts. In some cases, these elements are no longer extant or have been modified over time. Although enhancement projects may not include repair or reconstruction, protect support poles for canopies or balconies during construction (see "Protection of Historic Features during Construction" below). In addition, do not introduce new elements to the streetscape that will inhibit future restoration of historic canopies and awnings by building owners.

STREET FURNITURE

- Identify and protect historic street signs, planters or historic plantings, hitching rings, benches, and other historic street furniture and preserve these elements in their original locations.
- Concentrate new street furniture, such as benches, planters, and trash receptacles, at corners. Only include furniture where sidewalks are wide enough to accommodate these elements. Select furniture based on a local historic design, or introduce a simple, modern design that is compatible in scale, style, color, and texture with surrounding significant historic features. Do not introduce historic designs from other locations as this will present a false sense of local history. Choose muted colors for modern elements to avoid distracting attention from the historic features of the buildings and streetscape.
- Do not permanently fix new street furniture to historic features. Instead, connect furniture through pavement joints, with chains to stationary objects, or other reversible methods.

STREET LIGHTS

- Protect and incorporate existing historic lighting into the streetscape design where possible.
- If historic street lights are no longer extant, choose new fixtures that match the historic as closely as possible. Some manufacturers continue to produce historic designs. However, if a commercial district did not have street

lights during the historic period or if the community does not wish to replicate the historic lights, choose new street lighting that is a modern design but compatible in scale, appearance, and color with the character of the downtown. Do not select historic designs from other locations as this would create a false sense of historical development.

ARTWORK AND DECORATION

- Retain and preserve historic artwork such as murals and sculpture.
- Carefully consider proposed new decorative features for their compatibility with the historic district or buildings, and whether their design and construction are likely to stand the test of time. Plan for staff time, training, and funding for periodic maintenance of proposed new features.
- Consider limiting the number of new decorative features in an effort to retain the historic character of the area.
- Seasonal enhancements and decorations are generally acceptable, provided they do not damage historic fabric. Attach festive lighting and decorations to non-historic surfaces.

PROTECTION OF HISTORIC FEATURES DURING CONSTRUCTION

Historic features, such as buildings, fences, and canopies, should be protected from damage during sidewalk demolition and construction. At the request of the THC, the Texas Department of Transportation has produced standard protection notes, below. Insert these, or similar protection notes, in construction documents:

PROTECTION NOTES FOR THE REMOVAL OF EXISTING PAVEMENT, CURB OR SIDEWALK AND CONSTRUCTION OF NEW PAVEMENT, CURB OR SIDEWALK ADJACENT TO HISTORIC BUILDINGS, CANOPIES, MATERIALS, FENCES, AND RETAINING WALLS

Where proposed work is in proximity to historic buildings or other structures (walls, canopies, retaining walls, fences), and planting beds, and vegetation/groundcover, follow the procedures listed below for demolition, protection, and construction at these addresses:

In the city of _____, at _____(list addresses):

1. To minimize potential damage to historic structures and materials, contractor to saw cut existing sidewalk 8 to 12 inches away from the historic structure, canopy supports, fence, or retaining wall.
2. Contractor to construct new sidewalk next to the saw cut edge with installation of expansion joint in between. If existing sidewalk is to be removed entirely, the remaining 8 to 12 inches next to the historic structure, canopy supports, material, fence, or retaining wall will be removed by hand. Expansion joint to be placed between historic structure, canopy support, material, fence, or retaining wall and new sidewalk.
3. Contractor is responsible for preventing damage to historic structure, canopy supports and their awning, materials, fences, retaining walls, including garden elements (planting beds, plantings) during the entire construction project, especially during removal of existing pavement, curb, or sidewalk. During the saw cut and hand removal process, contractor will exercise utmost caution and will physically protect historic structure foundation, canopy supports, materials, elevations, entryways with decorative flooring, fences, retaining walls, and landscape elements.
4. Contractor to repair or replace in kind, at his own expense, any historic materials damaged in the course of executing the work. Contractor is responsible for locating replacement source for historic materials damaged in the course of the work. Texas Historical Commission to be informed of damage and proposed repairs prior to execution of repair work.

CONCLUSION

Providing a safe and pleasant streetscape environment is important but does not require the loss of historic context. Streetscape modifications can be accomplished while being sensitive to and enhancing local historic resources. Early and consistent consultation is the most effective way to assist the THC in providing a smooth and efficient review. Designs may need to be revised in order to comply with preservation standards—please do not wait until the construction documents are completed to involve the THC. *(Updated 2011.)*

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real places telling real stories

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